

HEREFORDSHIRE COUNCIL
PLANNING SERVICES
DEVELOPMENT CONTROL

04 OCT 2013

To:

Ack'd: File:

Heritage Statement

Project Name

Whitney on Wye, Toll House & Bridge

Project Number

2032

Client

Maggie Taylor & Grahame Penny

Date

August 2013

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SECTION 1: INTRODUCTION TO THE PROJECT AND PROJECT TEAM

1. The Applicant has instructed RRA Architects to submit a Listed Building Consent (LBC), application for the alteration and repair of an existing Grade 2 listed building at Whitney-on-Wye, Herefordshire. The proposed alterations relate to the range of buildings known as the Whitney-on-Wye Toll Bridge Cottage and Toll Bridge their structural repair, to maintain and provide an appropriate quality residential cottage, to meet the wide-ranging needs of the Toll Bridge, under the 1774 Act of Parliament, and the wider area, and provide a steady income stream to support the ongoing maintenance and upkeep of the listed building, the Whitney-on-Wye Toll Bridge and it's listed curtilage.
2. In terms of the composition of the project team, the following consultants have been working with the Applicant in formulating proposals for Whitney-on-Wye Toll Bridge Cottage and they are as follows:
 - RRA Architects – Hereford office: architects responsible for providing architectural services, lead consultant, building heritage strategy advice, masterplan proposals and submission of LBC, in close consultation with the Applicant and project team.
 - James Johnston – Responsible for Ecology survey and bat mitigation advice.
 - Jerry Ross – Arboricultural survey and advice.
 - BWB Consulting – Flood Risk Assessment advice.
3. This Heritage Statement (HS), sets out a comprehensive assessment of the proposed alterations to the listed building and makes the clear and unequivocal case for why listed building consent should be granted in accordance with the provisions of preserved policies within the Herefordshire Unitary Development Plan (UDP), the draft emerging policy of the Herefordshire Core Strategy and Chapter 12 of the National Planning Policy Framework – Conserving and enhancing the historic environment. With this in mind, this heritage statement accompanies the LBC Application and is structured as follows:
 - Section 2: Site Description, Surroundings and Historic Context
 - Section 3: Heritage Value, Character Defining Elements
 - Section 4: Conclusions

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- APPENDIX

4. In addition to this HS, there are a number of supplemental reports prepared by the Project Team. Where relevant, these are referred to in the HS.

SECTION 2: WHITNEY-ON-WYE TOLL BRIDGE COTTAGE: THE SITE, ITS SURROUNDINGS AND HISTORIC CONTEXT

Description of Whitney on Wye Toll Bridge Cottage and Historic Context

5. Whitney-on-Wye Toll Bridge Cottage is a Grade 2 listed building and it sits within the curtilage of the Whitney-on-Wye Toll Bridge, which is located at the junction of the B4350 and the A438. To the West is the settlement of Rhydspence, and to the East is the settlement of Winforton, and to the South is the settlement of Clifford. Figure 1.



Figure 1

6. The English Heritage description nos: 1157553, describes Whitney-on-Wye Toll Bridge Cottage as: *'WHITNEY ON WYE CP B 4350 (west side) SO 24 NE 5/114 Toll House on north-west side of Whitney Toll Bridge 12.3.81 (formerly listed as Toll Bridge and Toll House) GV II Toll house. Late C18 for Tomkyns Dew, altered and extended mid-C20. Part stuccoed scantling timber-framing, part painted brick with machine tiled roof with brick stack to rear of ridge. Originally a two-bay plan extended by one bay to the north and continuing the main roofline. Single storey and attic. Windows are all C20*

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casements. Main east elevation has two 2-light windows in the original part flanking the main entrance which has a gabled half-glazed timber porch. There is a 3-light window in the north extension. Notice boards attached to the front elevation provide details of the bridge's history, earlier and more recent toll rates. Attic light at south end. C20 extension to rear. The site for the construction of a toll house was provided as part of the Whitney Toll Bridge Act of 1780. Included for group value. (BoE, p 320; Garvett, D: Bridge Over the River Wye, 1984).'

7. The overall site edged in blue is 0.475ha and the site edged red is 0.419ha which forms the LBC application and Outline Planning Application site. The application site does not comprise works to the existing bridge, which is a listed building separately described by English Heritage, under listing nos: 1099494 and 1082465.
8. It should be noted that the Whitney-on-Wye Toll Bridge origins date back to the C18 and it is afforded its own special Act of Parliament, dated 1774, that governs the ongoing care and maintenance of the Toll Bridge and the associated buildings used in the support and the care of the bridge in perpetuity.
9. Sections of the C18 structure remain and form parts of the Bridge and Toll Bridge Cottage. Since the C18 the Toll Bridge Cottage and grounds have been adapted and altered substantially. The main Toll Bridge Cottage is constructed in local rubble stone with applied render and is laid out as a low level one and a half storey structure. The original Toll House stood adjacent to the Cottage on the opposite side of the toll road. This structure burnt down in the 1940's.
10. Whitney-on-Wye Toll Bridge and Toll Bridge Cottage has passed through many owners since the C18 and consequently alterations have occurred, which have resulted in some of the original detailing being removed and lost from the original cottage, with insensitive 1950's modernisation being applied to the interior and exterior of the buildings.
11. The Toll Bridge remains in tact in its original form, and as the Bridge does not form part of this application, no further description about the Bridge is felt necessary other than to say that the Applicant appreciates fully the Toll Bridge heritage and wishes to invest in the site to ensure that this important local and national heritage is preserved for future generations to enjoy and in accordance with the 1774 Act.

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12. The Toll Bridge Cottage is surrounded by outbuildings, which are arranged amongst connecting stone and concrete block garden walls and courtyards. The outbuildings comprise: - a garage / shed, a conservatory, a control house and a courtyard area.
13. The Garage / Shed – A single storey panel clad building constructed in part with concrete siding panels and timber panels, located on the West side of the Toll Bridge Cottage. This modern construction is poor quality and not worthy of conversion, or retention. The proposal seeks to replace this structure with a more appropriate design, described elsewhere, to preserve and enhance the setting of the listed building.
14. The Conservatory – A single storey timber frame single glazed lean-to building located on the South elevation of the Toll Bridge Cottage and linked into the dwelling via the lower ground floor room. The structure is contained by a low level garden stonewall, which connects to the Toll Bridge Cottage and to the side of the Toll Bridge wall. It has a single glazed roof with timber glazing rails. The applicant has part removed the conservatory glazing structure as it was unsafe – low level stone walls, in modern construction remain.
15. The Control House – Is a single storey timber kiosk building, which is located adjacent to the North elevation of the Toll Bridge Cottage, this structure forms part of the paraphernalia of the Toll Bridge workings; as it was used historically as a ticket collection and sheltering area for the Toll Master. This structure is a painted timber framed building and is clad in timber weather boarding. It has a duo-pitched roof clad with bitumen sheet.
16. The Courtyard – Is a large open area located on the East side of the Toll Bridge Cottage, separated by the toll road. This courtyard area is the site of the former Toll House that formed the complete collection of Whitney-on-Wye buildings. The Toll House that was located on this courtyard area burnt down in the late 1940's. The courtyard is bounded by low level stonewalls that extend from the site of the Toll Bridge. There are in addition timber picket rail fences that form the demise of the courtyard as it extends down towards the rivers edge.
17. Part of the areas surrounding the Toll Bridge Cottage require the use of a hard standing at river side level. Large length ladders, crawl boards and a boat are used from the hard standing by the Toll Master to access the Bridge abutments on a regular basis – trapped trees, pointing of stonework and the inspection of the underside of the Bridge – are a constant ongoing operational need. Adequate access and storage for this equipment is a

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vital requirement and a proposed new storage facility, described within a separate outline planning application, accompanies this LBC application.

The Toll Bridge Cottage – Internal Alterations

18. The LBC application seeks to remove and correct the erroneous modern alterations that have been applied to the listed heritage over time, in the pursuit of a more sympathetic approach, that seeks to alter, whilst preserving the heritage aspects of the building to be revealed more fully. The aim is to bring the principle spaces back into use and to improve the circulation from room-to-room throughout the Toll Bridge Cottage. The applicant intends to continue to use the building as their main dwelling and in time will allow the Toll Bridge Cottage to become a tourist facility to help support the revenue raising potential from the site – as tourists will favour staying within the traditional Toll Bridge Cottage – as opposed to the cotemporary Toll House structure – described separately.
19. Lower Ground Floor – The proposed alterations seek to remove a poorly proportioned modern glazed opening and replace it with a timber window / door set, which opens out onto the terrace area, where the conservatory was located. The existing modern stair is relocated, to be more efficient in terms of circulation. Beneath the stair the proposal seeks to include a WC and sink – hidden within a below stairs cupboard. The fireplace is retained along with the wood burning stove. The floor levels are corrected to remove the modern elevated timber flooring. So that the entire space will use lime mortar and a solid floor construction to allow any flooding event to be dealt with easily – for the washing out and the removal of silts and such like.
20. Upper Ground Floor / Kitchen – The bathroom space is removed from the modern extension area, to return the space to a single space – to be used as a kitchen dining area. The modern Crittal windows are replaced with timber framed opening casement widows. The South elevation includes the timber window / door frame set as part of the lower ground floor area – so that the window screen rises through two storeys and connects to an external terrace – which replaces the glass conservatory area.
21. Upper Ground Floor / Bathroom – Currently the space is used as the control room for the barrier to the Bridge. The internal timber partitions are modern in construction and are to be removed to allow a new wall partition to be constructed – to allow the space to be separated as a bathroom space.

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22. Upper Ground Floor / Reception Space – A new door way is formed to the side of the fire place and chimney breast, to improve the connectivity between spaces and to return the spaces to more regular spaces in plan – allowing the historic spaces to be appreciated in their original volume. The fireplace and wood burning stove is retained and the external porch, with access to the Bridge is retained. The proposed alterations seek the replacement of all modern Crittal windows with more sympathetic timber framed opening casements.
23. Upper Ground Floor / Bedroom Space – The proposed alterations seek to close off the modern entrance doorway openings and insert a new opening to connect with the main reception space. The proposed alterations seek the replacement of all modern Crittal windows with more sympathetic timber framed opening casements.
24. First Floor / Storage – The proposed alterations seek remove the upper floor spaces and create single volume spaces to ridge beam level. This will enhance the appreciation of the historic spaces and remove the modern staircase, which is not original and would not have formed parts of the original Toll Bridge Cottage. The proposal seeks to insert a deck area above the reception space, which will be used to house all the new plant equipment – such as the megaflo water storage and heating boiler system.

The Toll Bridge Cottage – External Alterations

25. East Elevation – The elevations will remain largely intact, with the historic sign boards protected as part of the historic vernacular. It is proposed that the windows will be replaced with more sympathetic traditional opening casement timber windows.
26. North Elevation – The proposed alterations seek to re-clad the 1950's modern extension to introduce a stained horizontal weatherboarding. This will help to reduce the mass of this 1950's addition so that it appears more subservient and sympathetic against the older structure of the Toll Bridge Cottage. A new timber framed porch will be added with a roof addition to shelter the doorway entrance to the kitchen area. The door will be reformed to correct the floor levels and stepped entrance. A new set of masonry steps will be formed to connect the upper ground floor with the lower ground floor levels externally.
27. West Elevation – The proposed alterations seek to re-clad the 1950's modern extension to introduce a stained horizontal weatherboarding. This will help to reduce the mass of this 1950's addition so that it appears more subservient and sympathetic against the

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older structure of the Toll Bridge Cottage. The sides to the original Toll Bridge Cottage will be re-rendered, with the exposed stone walling left exposed. A new terrace is formed to project over where the conservatory stood.

28. South Elevation – The proposed alterations seek to re-clad the 1950's modern extension to introduce a stained horizontal weatherboarding, within the main elevation of this 1950's extension it is proposed to insert a timber window and door to improve the proportion of the elevation and allow the Toll Master to have good views of the river and the Toll Bridge. The sides to the original Toll Bridge Cottage will be re-rendered, with the exposed stone walling left exposed. A new terrace, using lightweight construction and appearance, is formed to project outwards to the extent of where the conservatory stood. This introduces a private space that is less visible from the Toll road.
29. The roof will be re clad in slate and the rafter toes are to be exposed and renewed to remove the 1950's boxed eaves board – to return the property to more of its original detail and proportions.
30. The existing chimney will be reformed so that it corrects modern repair and the roof detail, which is not appropriately weathered, this will result in the stack being taken down and rebuilt, creating a more sympathetic brick chimney detail. The TV aerial will be removed and brought inside the property with an appropriate aerial booster.

SECTION 3: HERITAGE VALUE, CHARACTER DEFINING ELEMENTS

31. This Section of the HS sets out the planning policies that are relevant to the proposed application, and in so doing this section explores the enabling development that would be required as part of the development. This section is structured as follows:
 - Sustaining and Enhancing Our Cultural Heritage
 - Design
 - The Use of Lime and Traditional Construction at Whitney-on-Wye

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Sustaining and Enhancing Our Cultural Heritage

32. The proposed application is being developed in accordance with preserved UDP policies, the draft policy of Herefordshire's emerging LDF and Chapter 12 of the National Planning Policy Framework (NPPF).
33. Whitney-on-Wye Toll Bridge Cottage and Toll Bridge is not located within an AONB, or a Conservation Area, however the proposed development takes account of the important heritage elements of the site and landscape character and setting. The project team has ensured that the proposed development both sustains and enhances the essential integrity and coherence of the setting surrounding Whitney-on-Wye Toll Bridge Cottage and Toll Bridge.
34. The proposed development is limited to the extent of the existing buildings and as a result no archaeological investigation is required, this is confirmed in Pre-App letter received 3rd July 2012.
35. In accordance with chapter 12 of the NPPF the proposed development seeks to sustain and enhance the listed buildings. Some enabling/alteration development will be necessary to improve the accommodation to modern standards – although this is being limited to essential work. The planned alterations to the listed fabric are sensitive in nature. The design team has limited proposed alterations to an order of hierarchy as follows: structural need, complying with statutory legislation required as part of the Building Act, good sustainable design practice, correcting erroneous modern development and enabling new development to sustain and preserve important heritage – to ensure it's viability consistent with the buildings conservation for future generations to enjoy.
36. Given that the building is to be used for its original intended function, alterations are arranged to ensure the continuing viability of the Toll Bridge, (in accordance with the Act of 1774), to sustain and enhance the buildings to form a principle residence for the Toll Master and subsequent use as a holiday letting accommodation, to sustain an income stream to support maintenance of the Bridge. Following extensive business planning, tourism and new accommodation for the Toll Master – to free up the Toll Bridge Cottage for tourism, is the only viable use that is possible at the site, and the reader should refer to the Statement of Need – prepared by the Applicant. In all cases the building materials, proportions and detailing is in keeping with the original building and the requirements of the Act of 1774.

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37. The collection of modern poor quality structures, surrounding the site, are to be removed from the setting of the Toll Bridge Cottage. As part of the Outline Planning Application, which accompanies this LBC Application, it is proposed that the modern poor quality structures are replaced with a single low level contemporary building that will help sustain, and enhance, the Listed Building. The proposed low level development does not cause loss or harm to the listed building and will help to sustain the heritage by provided garaging, storage, ablutions and an improved sewage treatment facility.
38. In accordance with Draft Policy that is emerging within the Herefordshire Council's LDF process, the Applicant has had particular regard to Draft Policy RA.3, RA.4 and RA.5. The proposed LBC application helps to support the rural economy by creating and safeguarding jobs and this important local business. These policies accord also with the requirement to uphold at the site the 1774 Act of Parliament.

Design

39. The Applicant has referenced similar tourism and Toll Bridge project developments nationally and wishes to provide a high quality tourism business at Whitney-on-Wye. The proposed development will assist in boosting Toll Bridge revenue streams, to allow improved income to be channelled into the upkeep of the Bridge in perpetuity.
40. This type of historic toll house accommodation is unique within Herefordshire. As a result the Toll Bridge Cottage heritage of Whitney-on-Wye will help to inform the promotion of the tourism business and help sustain jobs and the local economy. The heritage aspects of the site are sustained and enhanced to ensure that proposed alterations retain the character and integrity of the original buildings.

The Use of Lime and Traditional Construction at Whitney-on-Wye

41. Reference is made to the company 'Ty Mawr' and their promotion of lime technology in traditional building construction and restoration. Appendix B. The following chapters set technical information from the Ty Mawr's archive. This establishes the proposed principles for the use of lime within the proposed development.
42. Lime, caring for moisture in buildings – Lime is vapour permeable and is the correct material for use at Whitney-on-Wye. Not only does lime based mortar and render absorb moisture from the surrounding brick and stone, it allows salt migration damage to occur in the lime and not in the structure of the stone or brick. This helps prolong the life of

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- the buildings for future generations to enjoy. In addition moisture can wick through the structure so that mould growth, or condensation, is limited.
43. Lime, correct choice in application – It is important to select the correct mortar strength for use in traditional buildings. The softness of the stone is determined to gauge the correct lime mortar specification. The existing lime mortar at the proposed alterations will be sampled and used to recreate the appropriate fat lime and hydraulic lime mortar recipe.
44. Lime render, internally – The proposed alterations requires the insertion of partition walls to divide the habitable space appropriately. Such partition walls will use traditional construction methods consisting of SW timber grounds with lathing. The lathing will receive a layered build up of lime render. The plaster selection will be based on strength requirements in accordance with Ty Mawr's table (See Appendix B). The render build up will occur in several layers with scratch coats and final coats determined by the lime plaster specialist. The use of partition walls will allow the accommodation of modern services and electrical conduits – reducing the need to recess services within the historic stonework. Where services on stonework can not be avoided this will be surface fixed in raised conduit.
45. Lime and the use of upgrading wall insulation to existing solid stonewalls – Modern legislation requires the upgrading of buildings for the conservation of fuel and power to meet with carbon reduction and the Kyoto protocol. Whilst this need to dry line walls with insulation products tends to degrade historic buildings – new products are available that work with traditional buildings and the requirements of lime based technology. The proposed alterations will seek a dispensation from the LABC in order to expose the vast majority of stonework to the interior. However some walls, particularly those of poor quality facing, will receive an insulated Calcium Silicate Board. This will achieve U-Values up to 0.77 W/m²K. For further reference on insulated board selection please refer to Appendix B.
46. Lime and the use of insulation within existing timber roof construction – The proposed alterations will use 'Thermafleece' in a warm roof construction. This material is a natural material comprising sheep's wool this means it is a breathable material and will allow the roof construction to breath and limit the occurrence of condensation and the potential for rotting of timberwork over time. This reduces the need for modern timber

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construction and allows the ceiling board to be lime rendered to achieve U Values up to 0.18 W/m²K.

47. Lime and new structural floor slab construction – The proposed alterations will use a lime based structural slab, to limit the need to excavate below the existing stonewall foundations. The typical build up of the structural slab is noted in the detail drawings, which accompanies this application. The material built up comprises:

- a levelled and compacted soil sub-base
- an insulating compacted hardcore layer of recycled 'foamglass'
- breather membrane and radon gas barrier
- A 'limecrete' slab
- A lime screed and floor finish comprising flag stone flooring

SECTION 4: CONCLUSIONS

48. The purpose of this Heritage Statement has been to set out a comprehensive case for the alterations of Whitney-on-Wye, to allow the site to continue to provide resources to maintain its future upkeep and provide a high quality environment for the Toll Master and tourists wishing to visit the area.
49. In so doing, this Heritage Statement has undertaken a thorough review of the heritage and conservation policies relating to the proposals, this clearly established that the proposed application sustains and enhances the building, which is Listed Grade 2.
50. The current Whitney-on-Wye building is no longer conducive to modern living and without alteration it is not suitable for supporting the business related to the upkeep of the Bridge in accordance with the Act of 1774. Therefore the building itself is a direct impediment to the further success of the Whitney-on-Wye Toll Bridge, despite the high quality of heritage, which is present there. Current planning and heritage policy supports the alteration of such buildings and seeks to ensure the ongoing use of the buildings and the sustaining and enhancement of the site as a whole. In that regard planning policy does not conflict with the legal obligations to comply with the 1774 Act of Parliament.
51. In proposing to carry out alterations to the building, it is necessary to test the principle of development against the provisions of planning policy. In this instance, because it is a

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Grade 2 Listed Building, the planning policy offers clear support for such alteration to a listed building. As part of the analysis, which supports development, because of the catchment area for heritage based tourism and because of the importance the Toll Bridge has within the local road network this site is not just an important local facility for Whitney-on-Wye – but for the whole of Herefordshire and beyond. It has to be stressed that the Whitney-on-Wye estate provides particular interest in Toll heritage, which is significant at this location – given the Act of 1774. In addition because of the significance of the Act the proposed development associated with this LBC application cannot be located elsewhere.

52. Having established that the principle of heritage led development is acceptable; it is then necessary to consider particular aspects of the proposals, the first being the heritage and conservation standards of the development. Clearly, given the 'careful adaptation' approach to the site, this presents a major opportunity for sustaining and enhancing the heritage found at the site. With this in mind, RRA has carefully considered all the options available, and now proposes a high-quality contemporary scheme that incorporates environmental technologies, enhances the heritage aspects of the site and sustains the character and setting of the listed building, in order to maintain heritage for future generations to enjoy, and to ensure contemporary architecture is introduced to a very high standard.
53. From a heritage and conservation perspective, enabling development and correction of erroneous modern repair and structural defects within the existing buildings will ensure the historic structures are sustained for future generations.
54. The presence of ecology on the site has been reported, but this will not affect the Listed fabric. Therefore development proposals will provide compensatory measures for ecology, which do not result in development work being required to the Toll Bridge Cottage.
55. From a landscape perspective, discussions have been held with the Council and it is understood that as the development is contained within the existing footprint of buildings, and avoids the SSSI, the proposals will have no negative impact on the surrounding landscape character, but will enhance the landscape, as the buildings are being restored and augmented with high quality design – to improve the setting within the curtilage of the listed buildings.

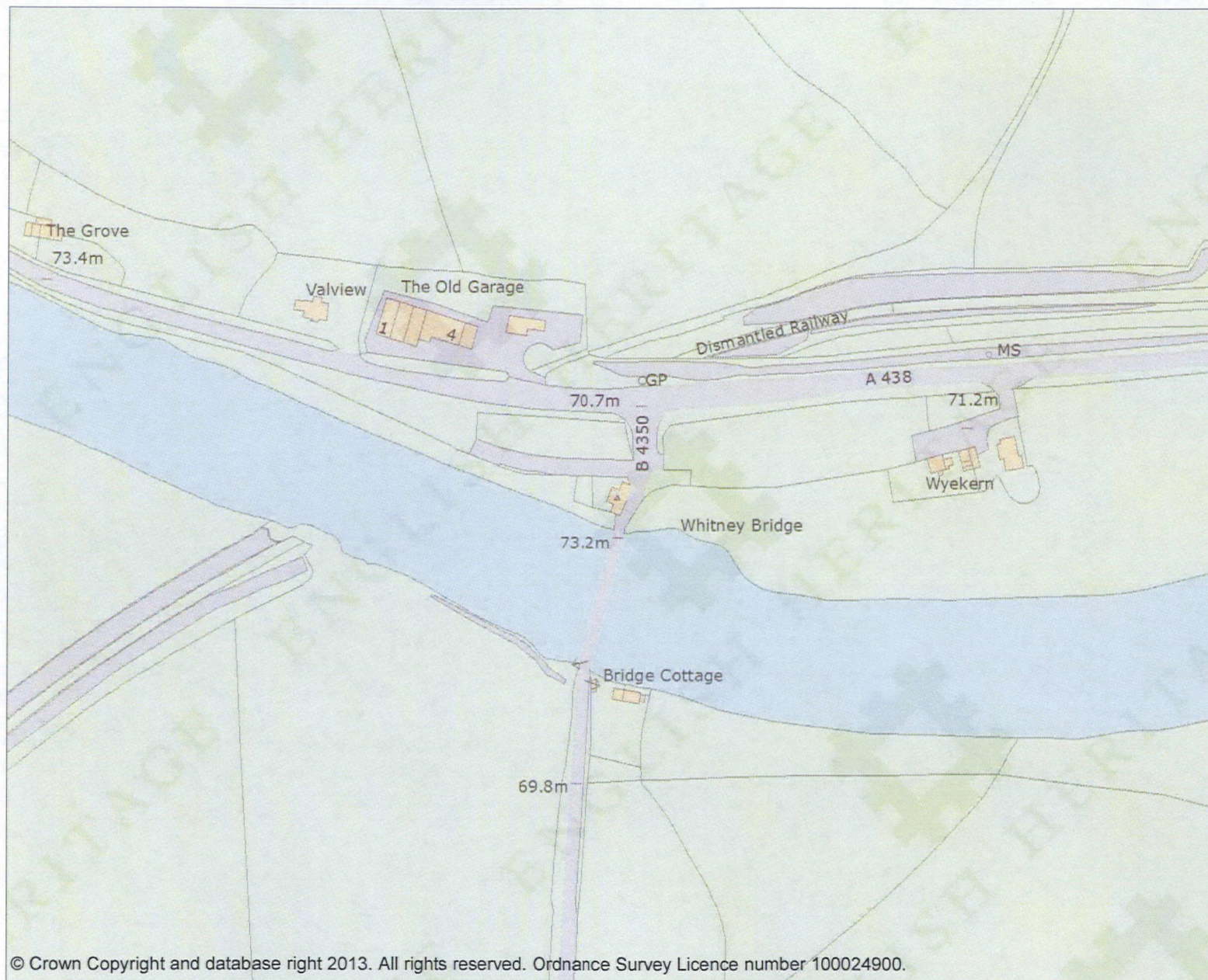
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Therefore, in conclusion, it is considered that the development proposals from a conservation and heritage perspective accord with the provisions of planning policy where relevant and LBC permission should be granted, subject to appropriate conditions, provided they do not impede the ability to uphold the 1774 Act of Parliament.

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APPENDIX A

English Heritage Listing Statement



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This is an A4 sized map and should be printed full size at A4 with no page scaling set.

Name: TOLL HOUSE ON NORTH-WEST SIDE OF WHITNEY TOLL BRIDGE

Heritage Category:

Listing

List Entry No :

1157553

Grade:

II

County:

District: County of Herefordshire

Parish: Whitney-on-Wye

For all entries pre-dating 4 April 2011 maps and national grid references do not form part of the official record of a listed building. In such cases the map here and the national grid reference are generated from the list entry in the official record and added later to aid identification of the principal listed building or buildings.

For all list entries made on or after 4 April 2011 the map here and the national grid reference do form part of the official record. In such cases the map and the national grid reference are to aid identification of the principal listed building or buildings only and must be read in conjunction with other information in the record.

Any object or structure fixed to the principal building or buildings and any object or structure within the curtilage of the building, which, although not fixed to the building, forms part of the land and has done so since before 1st July, 1948 is by law to be treated as part of the listed building.

This map was delivered electronically and when printed may not be to scale and may be subject to distortions.

List Entry NGR:

SO 25892 47468

Map Scale:

1:2500

Print Date:

30 May 2013



ENGLISH HERITAGE

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APPENDIX B

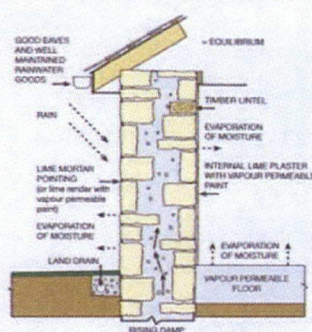
Ty Mawr – The Use of Lime Technology

Lime - Properties

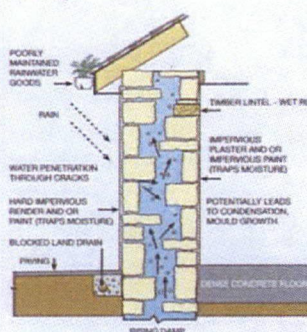
Caring for buildings

The importance of lime mortars, plasters and paints in caring for old buildings has now been well documented, this is because they have many desirable properties:

- Lime products are vapour permeable and can absorb moisture from surrounding bricks and stones. Any salt or frost damage occurs in the lime, thus protecting the surrounding materials.
- Lime products are capillary active (they are said to allow buildings to 'breathe'). This means that moisture can enter the wall but importantly it is then allowed to evaporate from the wall (unlike with most modern mortars and paints which hold moisture in the wall), this helps to naturally control condensation and damp within the building.



'Breathing' Solid Wall Construction
Building - maintained with soft, traditional, vapour permeable materials.



Solid Wall Construction - maintained with hard, modern impervious materials potentially trapping moisture, leading to problems of damp, condensation, mould growth, wet/dry rot etc.

Almost one quarter of the UK building stock is of solid wall construction. Care should therefore be taken with the way they are repaired and maintained. These old buildings are beautifully simple to understand and are relatively easy to maintain if the principles are understood (see above). The problem arises when 'inappropriate' materials are introduced and they are treated in the way we would treat modern cavity wall construction buildings. Care should be taken NOT to confuse the two technologies...

The Ty-Mawr Lime Handbook

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Choosing a lime

It is important for the well being of the building that the most appropriate lime product is chosen. Every situation needs to be carefully assessed e.g. in terms of:

- softness of the stone, brick etc – this should be a major factor in selection.
The mortar should always be 'softer' than the stone/brick, see table below.
- the **environmental conditions** – where the environment is very wet e.g. sea-defence walls, canal walls, cellars, copings, pavings; a hydraulic lime is more appropriate because of its ability to set in harsher conditions, see table below.

Lime mortar selection

Application	Type of Lime	Suggested Mix Ratio by volume	Notes
Pointing/Building Stone/Brickwork	Fat Lime Mortar	Premixed or 3 Sand*: 1 Lime Putty	<ul style="list-style-type: none"> ■ The exact ratio will depend on the sand/aggregate used. ■ The colour, texture, workability and success of the mortar is predominantly influenced by the selection of sand/aggregate. ■ The softer the stone/brick, the softer the mortar must be. ■ To match an existing mortar, send a sample to Ty-Mawr.
	Hydraulic Lime NHL3.5/2	2.5 Sand*: 1 Hydraulic Lime	
	glaston Lime Mortar	Premix	
Flag Stone Bedding	Hydraulic Lime NHL5/2	2.5 Sand*: 1 Hydraulic Lime	<ul style="list-style-type: none"> ■ For smaller tiles, please contact us. ■ Samples are highly recommended.
	glaston Bedding Mortar	Dry, Premix	
Paving, Copings Chimneys, Parapets	Hydraulic Lime NHL5/2	2 Sand*: 1 Hydraulic Lime	<ul style="list-style-type: none"> ■ For exposed areas, or any high weathering applications.

*It is important to choose a sharp, well-graded, well-washed sand. NHL = Natural Hydraulic Lime

- the existing materials – repairing like-with-like – this will usually provide the most satisfactory solution both aesthetically and technically, however it should not be done without **considering the performance of the original materials**.
- the time of year – the ideal time to do fat lime work, particularly external work, is during the spring and early summer, **lime work should never be done when the temperature is likely to fall below 5°C before carbonation has taken place**. Therefore, if work can not be rescheduled to another time of the year consideration should be given to adding a pozzolan to a fat lime (e.g. GGBFS, brickdust, calcined clay) to speed up the set or switch to using a hydraulic lime. Appropriate protection is vital.
- the available time – occasionally work is programmed without **enough time to support the use of the materials specified**, rapid builds necessitate rapid sets therefore adding a pozzolan to a fat lime or using a hydraulic lime instead should be considered (if work can't be reprogrammed!), but for best results application during autumn and winter months should be avoided.

The Ty-Mawr Lime Handbook

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Traditional lime plaster selection

Building Material*	Site Type	Suggested Base Coat / Levelling Coat	Suggested Build-up	Suggested Top Coat * please note the top coat should not be stronger than the base coat	Suggested Build-up
Cob, Rammed Earth, Strawbale*	Internal	Fat Lime Base Coat Plaster	2 x 9mm	Fat Lime Top Coat Plaster	1 x 3mm
(haired base coats)	External	Fat Lime Base Coat Plaster or Hydraulic Lime NHL2	2 x 9mm 2 x 9mm	Fat Lime External Top Coat Plaster or Hydraulic Lime NHL2	1 x 6mm 1 x 6mm
	Internal**	Fat Lime Plaster For Boards	2 x 9mm	Fat Lime Top Coat Plaster	1 x 3mm
Reed Mat, Reed Board (haired base coat)	External		N/A		
Celnet Wood Wool Boards (mesh base coat)	Internal**	Fat Lime Plaster For Boards (unhaired) or Hydraulic Lime NHL3.5/2 (with Beach aggregate)	2 x 6mm 1 x 6mm	Fat Lime Top Coat Plaster Hydraulic lime NHL3.5	1 x 3mm 1 x 6mm
	External	Hydraulic Lime NHL3.5 (with Beach aggregate)	1 x 9mm	Hydraulic lime NHL3.5	1 x 9mm
	Internal**	Fat Lime Plaster For Boards or Hydraulic Lime NHL3.5 (with Beach aggregate)	2 x 6mm 1 x 6mm	Fat Lime Top Coat Plaster Hydraulic lime NHL3.5	1 x 3mm 1 x 6mm
Woodfibre Board (mesh base coat)	External	Hydraulic Lime NHL3.5 (with Beach aggregate)	1 x 9mm	Hydraulic Lime NHL3.5	1 x 9mm
	Internal	Fat Lime Base Coat Plaster	2 x 9mm	Fat Lime Top Coat Plaster	1 x 3mm
Lath (internal only) Soft Stone (haired base coats)	External	Hydraulic Lime NHL3.5/2	2 x 9mm	Fat Lime Top Coat Plaster or Hydraulic Lime NHL3.5/2	1 x 6mm
Soft Brick (haired base coat)	Internal	Fat Lime Base Coat Plaster	2 x 9mm	Fat Lime Top Coat Plaster	1 x 3mm
	External	Fat Lime Base Coat Plaster or Hydraulic Lime NHL3.5/2	2 x 9mm 2 x 9mm	Fat Lime Top Coat Plaster or Hydraulic Lime NHL3.5/2	1 x 6mm 1 x 6mm
Hard Stone (haired base coats)	Internal	Hydraulic Lime NHL2	2 x 9mm	Fat Lime Top Coat Plaster	1 x 3mm
	External	Hydraulic Lime NHL3.5	2 x 9mm	Hydraulic Lime NHL3.5	1 x 6mm
Hard Engineering Brick Concrete Blocks (10mm mesh or haired base coat)	Internal	Hydraulic Lime NHL3.5/2	2 x 9mm	Fat Lime Top Coat Plaster or Hydraulic Lime NHL3.5/2	1 x 3mm 1 x 6mm
	External	Hydraulic Lime NHL3.5	2 x 9mm	Hydraulic Lime NHL3.5/2	1 x 6mm
Insulation Blocks* (10mm mesh or haired base coat)	Internal	Hydraulic Lime NHL3.5/2	2 x 9mm	Fat Lime Top Coat Plaster	1 x 3mm
	External	Hydraulic Lime NHL3.5	2 x 9mm	Hydraulic Lime NHL3.5/2	1 x 6mm

*Dub out uneven surfaces prior to applying first coat. **May require more coats due to waviness of bales.

*Insulation blocks have very high suction, be careful to maintain moisture content in render.

**Lime Hemp plaster is preferred in these scenarios. Please note: Manufacturers application guides must be followed.

To reduce the environmental impact of your lime plaster, use glasters or lime hemp plaster as alternatives to standard lime plasters. NHL= Natural Hydraulic Lime (to which plastering/rendering aggregate, must be added in correct ratio).

Exposed elevations may require additional coats.

Occasionally, the choice still seems to be based on the availability of skills (or lack of them) and this does mean that often in these cases that the **stronger hydraulic limes or even cement are introduced into buildings where they have no place** and will, in the long term, almost certainly cause damp problems and damage the very fabric of the building.

The next sections look at the various lime-based products in more detail and sets out some 'golden rules' for successful results. Before beginning to use any lime product, it should be understood that **eyes and skin should be protected at all times** – see safety sheet in appendix.

Types of Lime

Application guide - Plastering with traditional fat lime plaster

Golden rules

- Choose the right type of lime for your application, see page 26.
- Choose an appropriate well graded aggregate – important to the workability as well as success of the plaster.
- Use hair or mesh in base coats to give tensile strength, see page 26.
- Mix plasters thoroughly, do not add too much water.
- Thoroughly prepare the surface - do not remove old lime plasters/renders unnecessarily (often they can be patched or repaired).
- Ensure the background/ substrate is appropriate for lime work.
- Damp down the background before applying each coat (not if directly on to wood wool or woodfibre boards!).
- Always apply in even coats not greater than 8-9mm thick (base coat) and 3-4mm (top coat) - if the surface is uneven to start with then 'dubbing-out' is required, see page 31.
- Allow the backing coat to 'go off' hard before applying the next coat (N.B this can sometimes be a slow process). Use the 'Rule of Thumb' – Plaster should be hard enough that it cannot be indented by firmly pressing your thumb print into the plaster, but can still be indented by pressing your thumb nail into it.
- Do not allow lime plaster or render to dry too quickly – all coats should be allowed to 'carbonate' in the presence of moisture and Carbon Dioxide – therefore avoid dehumidifiers and heaters; work may also need to be sprayed to slow 'drying' down.
- Do not add cement or any other unspecified additives.
- Finish with a 'breathable' paint, see pages 69-83.
- Refer to full details on following pages, check the www.lime.org.uk for latest version of application guide.

Health and Safety Information



WARNING
Skin Irritation 2 H315 Causes skin irritation.
STOT SE 3 H335 May cause respiratory problems.

DANGER



Eye Damage 1 H318 Causes serious eye damage.

Precautionary Statements

P102 Keep out of reach of children. P280 Wear protective gloves, eye protection, face mask. P305 + P351 + P338 If in eyes rinse cautiously with water for several minutes and immediately get medical assistance. P352 + P352 If on skin, wash affected parts immediately with plenty of soap and water.

Common mistakes/troubleshooting

If you are experiencing problems (cracking, crumbling, dusting, failure) – please refer to our 'Troubleshooting' section on the website www.lime.org.uk for possible causes and preventative and remedial actions.

The Tŷ-Mawr Lime Handbook

Lime mortars

Description

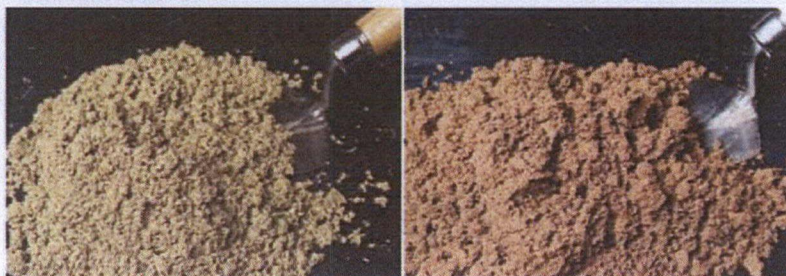
Tŷ-Mawr Premixed Lime Mortars are made from a high calcium lime (also known as a fat /air/putty or non-hydraulic lime) and carefully selected aggregates. Lime mortars require exposure to Carbon Dioxide in the presence of moisture to harden.



Components of premixed 'wet' mortars – well graded, sharp sand and fat lime putty

Colour and texture

Mortars can be seen in a wide range of colours and compositions across the country, they will vary from region to region as local sands, local sources of lime and pozzolans would traditionally have been used. It is the aggregate/sand that predominantly effects the colour and texture.



Different aggregates effect the colour and texture of the mortar

The Tŷ-Mawr Lime Handbook

Toll House – Whitney on Wye

Heritage Statement

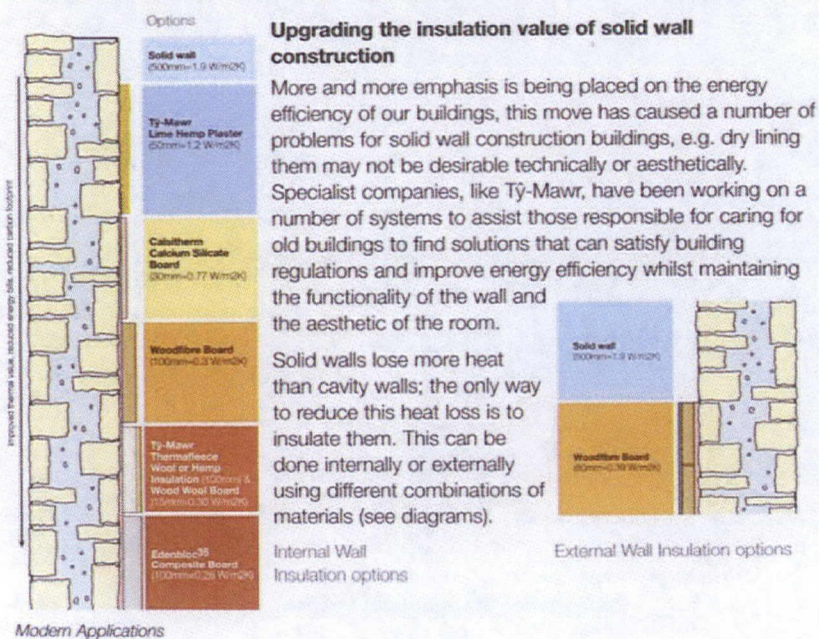
May 2013

Other applications

Lime is now being incorporated into a wide range of 'modern' applications that assist those caring for old buildings of all types and sizes as well as those involved in new ecological builds.

For upgrading old buildings, it is possible to use insulation products combined with lime that keep the building "breathing", however, it should be noted that any solution to upgrade the walls, floor or roofs within an old building should be assessed and carefully considered. Businesses, like Ty-Mawr, are able to undertake U-Value analysis and also static and dynamic modelling to determine moisture behaviour in a given scenario, hence predicting any interstitial moisture problems. This is very important if thinking of installing insulation internally on solid walls.

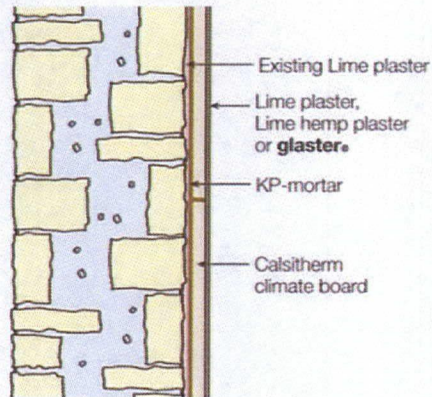
Prior to installation of the insulation solution, a detailed assessment of the condition of the wall should be undertaken. Any remedial actions undertaken prior to installation of insulation to avoid problems in the future. Careful consideration should be given to the method of fixing and installation by an architect to avoid problems such as thermal bridging.



Toll House – Whitney on Wye

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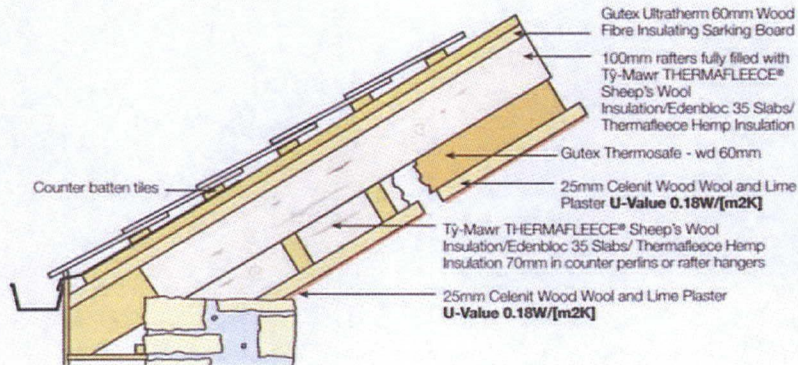


Internal Wall Insulation - example

A new and popular example of IWI is the calcium silicate board, this board can be directly adhered to the internal lime plaster and then plastered with a standard lime or lime hemp plaster. This solution has rapidly gained popularity as it adds insulation to the wall, maintains vapour permeability, and keeps the benefit of thermal mass of the solid wall without compromising too much space internally – see detail opposite.

Warm roof system

Similarly, this example shows that by using a combination of 'breathable', environmentally friendly boards and insulation products, along with lime plaster, a highly efficient warm roof system can be designed that also helps to regulate the humidity of the room. It also has a decrement value that helps to even out the peaks and troughs in the temperature throughout the day. It is therefore ideal for a living/working environment.



The Ty-Mawr Lime Handbook

HEREFORDSHIRE COUNCIL
PLANNING SERVICES
DEVELOPMENT CONTROL

04 OCT 2013

To:.....

Ack'd:..... File:.....

Planning Statement

Project Name

Whitney on Wye, Toll House & Bridge

Project Number

2032

Client

Maggie Taylor & Grahame Penny

Date

August 2013

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SECTION 1: INTRODUCTION TO THE PROJECT AND PROJECT TEAM

1. The Applicant has instructed RRA Architects to submit an Outline Planning Application – for a masterplan of development with all matters reserved; and a Listed Building Consent Application (LBC), – for the alteration and repair of an existing Grade 2 listed building at Whitney-on-Wye, Herefordshire. The proposals relate to the range of buildings known as the Whitney-on-Wye Toll-Bridge-Cottage and Toll Bridge. The proposed development is necessary to meet the wide-ranging needs of the Toll-Bridge – under the 1774 Act of Parliament; and to provide a steady income stream to support the ongoing maintenance and upkeep of the Whitney-on-Wye Toll-Bridge and listed heritage.
2. In terms of the composition of the project team, the following consultants have been working with the Applicant in formulating proposals for Whitney-on-Wye Toll-Bridge-Cottage and masterplan; they are as follows:
 - RRA Architects – Hereford office: architects responsible for providing architectural services, lead consultant, building heritage strategy advice, masterplan proposals and submission of LBC, in close consultation with the Applicant and project team.
 - James Johnston – Responsible for Ecology survey and bat mitigation advice;
 - Jerry Ross – Arboricultural survey and advice;
 - BWB Consulting – Flood Risk Assessment.
3. This Planning Statement (PS), sets out a comprehensive assessment of the proposed development and makes the clear and unequivocal case for why Outline Planning Consent should be granted in accordance with the provisions of preserved policies within the Herefordshire Unitary Development Plan (UDP), the Draft Emerging Policy of the Herefordshire Core Strategy, and Chapter 12 of the National Planning Policy Framework – Conserving and Enhancing the Historic Environment. With this in mind, this planning statement accompanies the Outline Planning Application and is structured as follows:
 - Section 2: Site Description, Surroundings and Context
 - Section 3: Description of the Proposed Development

Toll House – Whitney on Wye

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- Section 4: The 1774 Act of Parliament and Relevant Planning Policy
 - Section 5: Conclusions
4. In addition to this PS, there are a number of supplemental reports prepared by the Project Team – where relevant, these are referred to in the PS.

SECTION 2: WHITNEY-ON-WYE TOLL-BRIDGE-COTTAGE: THE SITE, ITS SURROUNDINGS AND CONTEXT

Description of Whitney on Wye Toll-Bridge-Cottage and Context

5. Whitney-on-Wye Toll-Bridge-Cottage is a Grade 2 listed building and it sits within the curtilage of the Whitney-on-Wye Toll-Bridge, which is located at the junction of the B4350 and the A438. To the West is the settlement of Rhydspence, and to the East is the settlement of Winforton, and to the South is the settlement of Clifford. Figure 1.

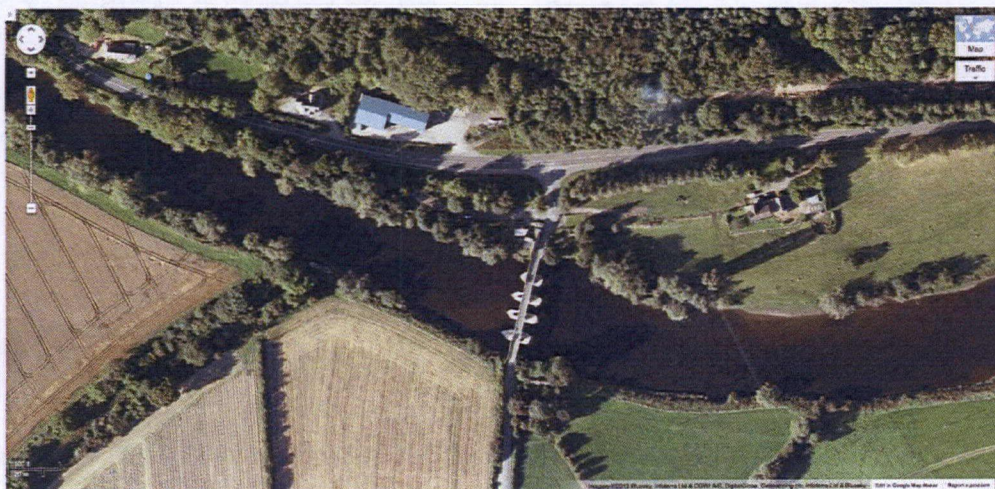


Figure 1

6. The overall site is 0.475ha and the application site (edged red on the attached location plan), is 0.419ha, which forms the Outline Planning Application and Listed Building Consent Application. The application site does not comprise works to the existing Bridge.
7. It should be noted that the Whitney-on-Wye Toll-Bridge origins date back to the C18 and it is afforded its own special Act of Parliament, dated 1774 – that Governs under Statute

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the ongoing care and maintenance of the Toll-Bridge and the associated buildings used in the support and the care of the Bridge in perpetuity. The detail of this Act is described elsewhere within the supporting documentation accompanying this application.

8. The Toll-Bridge-Cottage is surrounded by outbuildings, which are arranged amongst connecting stone and concrete block garden walls and courtyards. The outbuildings comprise: - a garage / shed, a conservatory, a control house, ablutions facility, a courtyard area and a landscaped area to the West of the Cottage.
9. The Garage / Shed – A single storey panel clad building constructed in part with concrete siding panels and timber panels, located on the West side of the Toll-Bridge-Cottage. This modern construction is poor quality and not worthy of conversion, or retention. The proposal seeks to replace this structure, which is used as an ablutions building for campers and fishermen, with a more appropriate design – described in Section 3.
10. The Conservatory – A single storey timber frame single glazed lean-to building located on the South elevation of the Toll-Bridge-Cottage and linked into the dwelling via the lower ground floor room. The structure is contained by a low level garden stonewall, which connects to the Toll-Bridge-Cottage and to the side of the Toll-Bridge wall. It has a single glazed roof with timber glazing rails. The applicant has part removed the conservatory glazing structure as it was unsafe – low level stone walls, in modern construction remain.
11. The Control House – Is a single storey painted timber kiosk building, which is located adjacent to the North elevation of the Toll-Bridge-Cottage, this structure forms part of the paraphernalia of the Toll-Bridge workings; as it was used historically as a ticket collection and sheltering area for the Toll Master. This painted timber framed building is clad in timber weatherboarding. It has a duo-pitched roof clad with bitumen sheet.
12. The Courtyard – Is a large open area located on the East side of the Toll-Bridge-Cottage, separated by the Toll road. This courtyard area is the site of the former Toll House that formed the complete collection of Whitney-on-Wye buildings – all of which were built in accordance with the 1774 Act of Parliament. The Toll House that was located on this courtyard area burnt down in the late 1940's, and the Toll House has not been replaced. The courtyard is bounded by low level stonewalls that extend from the side of the Toll Bridge. There are in addition, timber picket rail fences that from the demise of the courtyard as they extend down towards the rivers edge.

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13. Part of the areas surrounding the Toll-Bridge-Cottage require the use of a hard standing at riverside level. Large length ladders, crawl boards and a boat are used by the Toll Master on a regular basis from this hard standing to access the Bridge abutments – trapped trees, pointing of stonework and the inspection of the underside of the Bridge are a constant ongoing operational need. Adequate access and storage for this equipment is a vital requirement. It is currently stored externally and clutters the site. A proposed new storage facility is described in Section 3.
14. The Open Area and Camping Ground – this area is to the West of the Toll-Bridge-Cottage and extends into a densely wooded area. It is bounded by the A438 road – to the North, and the river Wye – to the South. Formerly this area was bisected by the old Hay on Wye road and the railway bridge abutment – this historic local infrastructure has now been removed, although a second bridge abutment is still standing and can be seen on the opposite side of the river. The open area is currently bisected by a 'tarmac' road, which extends onto the SSSI and SAC areas.
15. Adjacent to the camping ground and along the rivers edge, hard-standing areas exist, which are used for access by fishermen and for canoe launching – both activities help to sustain income for the Toll Bridge. The open area is used for camping and the pitching of tents and caravans for over night accommodation. This is a seasonal use and there are 'peaks and troughs' throughout the year.
16. The Toll-Bridge-Cottage – is described more fully within the LBC application and the Heritage Statement, which accompanies this application.

SECTION 3: Description of the Proposed Development

17. The proposed outline development can be described in three broad areas:
 - A new garage, store, visitor-kiosk, ablutions-building and sewage-treatment facility;
 - Four Eco-Lodges for use by fishermen and tourists as sleeping accommodation;
 - A new 2 bedroom Toll House to replace the Toll House lost in the 1940's.
18. This type of historic Toll Bridge is unique within Herefordshire. As a result this important local heritage will help to inform the promotion of the tourism business and help sustain jobs and the local economy and support income generation for the Bridge. The heritage aspects of the site, as a result of the proposed development, are sustained and

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enhanced to ensure that proposed alterations retain the character and integrity of the original buildings.

19. The Applicant proposes to limit the development of the Toll-Bridge-Cottage to alterations only, allowing this building to be used as a holiday letting accommodation. This will help to sustain income generated from the site (refer to the Statement of Need). This is in accordance with the advice from the Conservation Officer, who suggested that a new build Toll House would be preferable compared with the substantial extension and remodelling of the Toll-Bridge-Cottage that was proposed during the Pre-Application stage.
20. It is understood that camping and pitches for caravans are an established use at the site. Advice contained in Pre-Application suggests that this established use may be Permitted Development; or as the Applicant claims – is an established use at the site evolved over substantial time – and certainly longer than 10 years. Given the importance of the local heritage, and a need to generate income in accordance with the 1774 Act, camping is a tourism use that has evolved at the site over time. In order to satisfy the Local Authority's desire to recognise the 'Legality' of camping at the site it is understood to be a material consideration. Therefore the Applicant proposes 4 number Eco-Lodges (reduced from 6 submitted as part of the Pre-Application), to improve the quality of camping at the site and to further limit the number of caravan pitches being used in the future – which would otherwise clutter the site and be detrimental to the landscape character and setting of a listed building. The proposed Eco-Lodges are high quality bespoke designs that use natural materials to sustain and enhance the landscape character and setting. The lodges are stilted to reduce foundation impact and as a result of the removal of the tarmac road the development enhances bio-diversity – as confirmed by Natural England.

Design

21. The proposed new development is contemporary in style and appearance – as it intends to use natural cladding materials such as; wood, green walls and Sedum roofing. To maintain a low impact feel and design contrast with important local heritage the Applicant does not want to create a design-pastiche adjacent to what is a nationally important local heritage asset.
22. The site is next to, and benefits from, the SSSI and the river Wye SAC. Given these two important environmental and ecological references an ecology report has been

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commissioned and Natural England has been consulted as part of the pre-application process. The conclusion of this advice is:- the proposed development maintains a 10m buffer zone along the rivers edge, and the proposed building creatively uses foundation stilts and narrow piled foundations to minimise ground and root ball disturbance. As a result the development makes a positive bio-diversity contribution to the SSSI and SAC.

Eco-Lodges

23. Each of the four Eco-Lodges can provide a tea point, sleeping accommodation and a small shower & toilet facility. The aim of the Eco-Lodges is to provide a 'glamping' facility – to improve the current offering and to provide an improved income stream to support the Toll Bridge. The proposed high quality Eco-Lodges will help to reduce the clutter of caravans, which would otherwise be necessary to support continuing income from the site; the Eco-Lodges will help to raise and support tourism within the local area and promote the history of this important heritage asset within Herefordshire and beyond.
24. The Eco-Lodges are Cedar clad, and whilst they have a minimal glazed area, the windows use highly reflective glass such that the over all appearance is to reflect and melt into the landscape (see the attached Design and Access Statement). In time the lodges will take on a grey weathered appearance and the Sedum roof will grow flowers and grasses that will further blend into the landscape.
25. The existing 'tarmac' road will be removed and this hard surface returned to a softer natural porous surface – to improve site drainage and bio-diversity.

Ablutions Garage and Store

26. The proposed ablutions building & garage store is a flat roofed structure. It has a Sedum clad low level roof that provides a sheltered space, which connects the kiosk area and garage store together over a partially sheltered forecourt area – so that the over all appearance is to unify the structure and remove the clutter of buildings that exists currently.
27. The ablutions building places the lower ground floor storey beneath the garage area, so that the showers and toilets and sewage treatment facility are partially sunken and hidden from view. The natural slope of the land assists in concealing the lower ground storey within this area, and as a result reduces the clutter and the over all height impact upon the listed heritage.

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Toll House

28. The proposed Toll House is a two-storey structure with a Sedum roof. The building would be clad in Cedar boarding and vertical green walling. The principle first floor accommodation is stilted and cantilevered above the ground level, which according to Natural England does not adversely affect the SSSI or the SAC. The ground floor storey is stepped 10m back and located closer to the existing access gate – this elevation ties into the existing stone wall and continues an historic precedent of the Old Toll House being connected to the Bridge.

Toll Barrier

29. The Toll Barrier – is to be relocated and moved away from its current location and closer to the A438. This will allow traffic to queue and pass and wait safely. It also removes the potential hazard to pedestrians emerging from the existing door opening of the Toll-Bridge-Cottage. This does not require planning permission; or LBC approval, as confirmed by the Local Authority in email dated 8th November 2012, stating... *'Further to our meeting [on behalf of the LPA] I can confirm that the installation of new pay machines and associated infrastructure would constitute permitted development by virtue of Schedule 2, Part 20, Class A of the Town & Country Planning (General Permitted Development) Order 1995, and as such planning permission would not be required.'*

Kiosk

30. The Visitor Kiosk – is to be located further into the site towards the West. The new kiosk will house the historic paraphernalia of the Bridge and Toll activities and provide refreshments and information to tourists whilst visiting the site. The kiosk will accommodate the control room equipment and the facilities necessary to work the Toll Bridge and meet the ongoing needs of visitors that wish to use the site.

SECTION 4: The 1774 ACT of Parliament and Relevant Planning Policy

31. This Section of the PS sets out the planning policies that are relevant to the proposed application, and references the advice contained within the Pre-Application response dated 3rd July 2012. In so doing Section 4 explores the planning policy that supports the proposed development.

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32. A Pre-Application was carried out in summer 2012 and advice was forthcoming from Mr. Andrew Banks – Planning Office; Mandy Neill – Landscape Office; Bridgit Symons – Ecology Officer; and Robert Walker – Conservation Officer; this advice can be summarised as follows:
- A 10m section along the rivers edge is subject to a SSSI and SAC;
 - The Toll-Bridge-Cottage and Toll Bridge are Grade 2 listed buildings;
 - Relevant planning policy is: – Policy HBA1, NPPF Chapter 12, Policy RST14, Part B of General Permitted Development Order 1995, Policy LA2, BS5837, Policies NC1, NC6, NC7, NC8, NC9;
 - A Business Case needs to be defined;
 - Planning Obligations may be required;
 - A FRA may be required.
33. Given the above advice the Applicant is submitting an Outline Planning Application with all matters reserved and a LBC Application, to limit their exposure to costs – associated with the preparation of reserved matters detail. It is felt this method of approaching the procurement of development at the site is necessary to establish the principle on which development can occur at the site and to raise the general level of understanding of what liberties the 1774 Act of Parliament affords the Applicant – relevant to the Policy defined above.
34. The Applicant is mindful, from recent past activities to defend their rights under the 1774 Act, that there is a lack of understanding of the 1774 Act by the Public and the Local Authority, which has resulted in substantial costs being diverted from normal activity to safeguard the Bridge. Clearly as a result of this activity there is a need to 'educate and inform' the wider public about why development is needed, and how the Local Authority can support this important local heritage. An Outline Application with all matters reserved is therefore the preferred route to achieving these aims. Once the masterplan is approved it is understood that further detailed planning applications will follow.
35. Whilst Whitney-on-Wye Toll-Bridge-Cottage and the Toll Bridge is not located within and AONB, or a Conservation Area, they are Grade 2 Listed and located next to a SSSI and a

Toll House – Whitney on Wye

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- SAC. The proposed development therefore takes account of these important environmental and heritage aspects of the site and landscape character and setting.
36. The proposed development takes on board the Pre-Application advice and the Pre-Application proposals are amended such that the development is in accordance with preserved UDP policies HBA1, the Draft Policy of Herefordshire's emerging LDF and Chapter 12 of the National Planning Policy Framework (NPPF). These important planning policies do not conflict with the 1774 Act of parliament; or the uses established at the site, which are necessary under the Act to generate income to support the Bridge in perpetuity.
37. In accordance with chapter 12 of the NPPF the proposed development seeks to sustain and enhance the listed buildings and as a result of the Pre-Application advice the Applicant has limited the development of the existing Toll-Bridge-Cottage to alterations and repairs, and proposes to erect the New Toll House adjacent to the Cottage on the site of the former Toll House that burnt down in the 1940's. This accords with the advice of the Conservation Officer and Policy HBA1.
38. Given that the building is to be used for its original intended function, alterations are arranged to ensure the continuing viability of the Toll-Bridge – to sustain and enhance the buildings to form a principle residence for the Toll Master and holiday letting accommodation to augment revenue. Following extensive development of the Business Case tourism is the only viable supporting development use that would be possible at the site, and the reader should refer to the Statement of Need – where this is set out in more detail.
39. The Applicant confirms that camping and caravanning has occurred at the site for a substantial period of time in order to support income levels to assist in the upkeep of the Bridge. Whilst it is understood that in planning terms camping and tourism development at the site may not be recorded historically it is clear that this use is established at the site. In addition to the above the 1774 Act supports such development if it is necessary to sustain an income for the Toll Master in order to support the repair and maintenance of the Bridge in Perpetuity. Looking more specifically at UDP Policy RST14 the proposed Eco-Lodges would be chalet type accommodation located within a well-screened site with minimal impact on the surrounding landscape character and setting. The proposed scale of development is limited to 4 number lodges – from the initial 6 that were proposed within the Pre-Application. The site is well defined and laid out such that

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circulation and the accommodation within the existing trees helps to sustain and enhance the landscape and bio-diversity. Traffic generated is no more or less than the current established use at the site. The Eco-Lodges are limited in scope and could not be used as residential accommodation. The Eco-Lodges are elevated above the effective flood plain such that they accord with policy DR7 as the development is necessary to sustain important local infrastructure. Reference should be made to BWB Flood Risk Assessment.

40. In accordance with UDP Policy LA2 the proposed development has been reduced in scale to limit the affects within the overall character of the landscape. Eco-Lodges have been reduced in overall number from 6 to 4 and it is demonstrated within the Design and Access Statement how views from the South are sustained and enhanced by sympathetic design that is natural in appearance and reflective. (As confirmed in design precedent), Eco-Lodges present a narrow glazed transparent elevation towards the riverfront. The Toll-Bridge-Cottage is no longer being extended and has been limited to alterations only; this further preserves and enhances the landscape character and setting. All proposed new development is stilted to ensure that the SSSI is unaffected – as confirmed by Natural England. As a result in accordance with Policy LA2 the proposal demonstrates that the landscape character has influenced the design, scale, nature and site selection.
41. In accordance with BS5837 the proposed development takes full account of existing trees – as the Eco-Lodges and the New Toll House are located to avoid tree roots and canopy spread. The existing tarmac road is removed to improve site run-off and bio-diversity across the site and a new access road will use a porous surface. Refer to Jerry Ross's Arboriculture Survey and Report.
42. The environmental and ecological aspects of the development have been addressed through the advice of Jerry Ross, James Johnson and Natural England. In accordance with Pre-Application advice the Consultant reports demonstrate that the proposed development will enhance bio-diversity and does not adversely affect flora and fauna at the site, and as regard the development is in accordance with NPPF and UDP policies NC1, NC6, NC7, NC8, NC9. Consultant reports are appended to this application for further reference.
43. In addition to the Policy advice, set out within Pre-Application advice, reference is made also to Draft Policy that is emerging within the Herefordshire Council's LDF process. The

Toll House – Whitney on Wye

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August 2013

Project Team has had particular regard to Draft Policy RA.3, RA.4 and RA.5. It is noted that whilst this Draft Emerging Policy is a material consideration the proposed Outline Application helps to support this Draft Policy as:- it will sustain the rural economy by creating and safeguarding jobs and this important local business. These Draft Policies accord also with the requirement of the 1774 Act of Parliament. Therefore there is no statutory conflict between: the 1774 Act of Parliament, the preserved UDP Policy and the Draft Policy of the LDF.

44. A Flood Risk Assessment (FRA) / Desktop study has been prepared and accompanies this application. It is noted that the Eco-Lodges and the new tollhouse are set at a level above the effective flood plain. To meet planning policy requirements the applicant will implement the flood mitigation strategy in accordance with the advice contained within the supporting statements contained within the FRA. The proposed development maintains dry access and flood compensatory measures can be implemented at the site. It is understood that this detail information will be forthcoming during a reserved matters application. The FRA finds there are no impediments as to why outline-planning consent should not be recommended for approval subject to further detail design.
45. The proposed development seeks to upgrade and install an improved sewage treatment plant at the site. In addition, as a result of a substantial reduction of hard standing and the inclusion of Sedum roofs and drainage swales on all new proposed buildings – site run off will be substantially improved from its current level. These improvements are in accordance with policy to reduce and mitigate high Phosphate levels within the Wye catchment. It is therefore felt that the proposed development is in accordance with the Environment Agency Directive to limit Phosphate run off.
46. Planning Obligations have been highlighted as a planning requirement under the Council's Planning Obligations Supplementary Planning Document. Reference is made to the 1774 Act of Parliament where it is apparent that the proposed development site is free of all taxes and charges such that income can be sustained for the support and maintenance of the Bridge in perpetuity. On that basis the Applicant defends their rights under the Act, and it is understood that the planning application can be progressed on that basis.

SECTION 4: CONCLUSIONS

Toll House – Whitney on Wye

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47. The purpose of this Planning Statement is to set out a comprehensive case for the proposed Outline development of Whitney-on-Wye, to allow the site to continue to provide resources to maintain upkeep of the Bridge and to provide employment and a high quality environment for the Toll Master and tourists wishing to continue to visit the area.
48. In so doing, this Planning Statement has undertaken a thorough review of the planning policies relating to the proposals. It is established that the proposed Outline Application sustains and enhances the existing buildings and supports tourism and the creation of employment within the local area.
49. The current site at Whitney-on-Wye is no longer conducive to modern living and without development it is not suitable for supporting the business relating to the upkeep of the Bridge – to meet the needs of the Toll Master in service of the Act of 1774. Despite the high quality of heritage found at the site the buildings themselves are a direct impediment to the further success of the Whitney-on-Wye Toll-Bridge. Current planning and heritage policy supports the alteration of such buildings and seeks to ensure the ongoing use of the buildings and the sustaining and enhancement of the site as a whole. In that regard the proposed development does not conflict with planning policy; and the proposed development satisfies the legal obligations written within the 1774 Act of Parliament.
50. The Toll-Bridge is a vital local road infrastructure network, not just an important local facility for Whitney-on-Wye, but for the whole of Herefordshire and beyond – attracting many local users and emergency services that rely on the Bridge being open on a 24 hour basis. Should planning permission not be granted – the future of the Toll Bridge is uncertain, and it is likely to fall into increasing disrepair, or may be forced to close. Failure to grant planning permission for development would be a detrimental step, as it would either result in failure of the infrastructure in the very near future; or require substantial public funds to bring this vital infrastructure back into public use. In the past, Herefordshire Council declined to acquire this vital infrastructure site and under current budget constraints the Council is unlikely to do so in the future – therefore whilst private investment is seeking to develop the site sympathetically and in accordance with planning permission, the Local Authority is encouraged to fully support this application.
51. Given the Act of 1774 the Whitney-on-Wye estate provides particular interest in Toll heritage, which is significant at this location. In addition because of the significance of

Toll House – Whitney on Wye

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August 2013

the Act the proposed development associated with this LBC application cannot be located elsewhere and is seen as a vital piece of local history and infrastructure within the locality. These special circumstances require the development team and the Local Authority to view the application as a 'special case' where planning policy is interpreted favourably – in order that the proposed development can be brought forward successfully.

52. Having established that the principle of development is acceptable both in terms of Planning Policy and the requirements of the 1774 Act, with this in mind RRA Architects has carefully considered all the options available and proposes a high-quality contemporary scheme that incorporates environmental technologies, enhances the heritage aspects of the site and sustains the landscape character and setting of the listed building. As a result the proposed Outline Application and LBC Application will sustain and preserve this important local heritage for future generations to enjoy.
53. In addition to supporting local heritage the development proposals do not have any negative impacts upon ecology, as the proposed Outline Application enhances biodiversity at the site. This is confirmed in, the Ecology report, Arboriculture report and in consultation with English Nature.
54. From a landscape perspective the proposals will have no negative impact on the surrounding landscape character, as the buildings are being restored and augmented with high quality design – improving the setting within the curtilage of important local heritage and listed buildings.

Therefore, in conclusion, it is considered that the outline development proposals from; a design, conservation and heritage perspective, accord with the provisions of planning policy where relevant and Outline Planning Consent and LBC Consent should be granted, subject to appropriate conditions – provided they do not impede the ability to uphold the 1774 Act of Parliament.



Statement of Need

Project Name

Whitney on Wye, Toll House & Bridge

Project Number

2032

Client

Maggie Taylor & Grahame Penny

Date

August 2013

Hereford

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Whitney-on-Wye Bridge – A Proposal

Justification Statement of Need

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SECTION 1: INTRODUCTION

1. The Applicant has instructed a consultant team to submit an outline planning application for the redevelopment of Whitney-on-Wye Bridge. The redevelopment proposals involve the alteration of the Grade 2 listed toll house and the development of the surrounding land as a collection of sensitive, holiday eco-cabins, a new toll masters house and ancillary buildings to meet the wide-ranging tourism needs of the area and provide a secured and steady income stream for the ongoing maintenance and investment in the Whitney-on-Wye Toll Bridge in accordance with the Act of Parliament dated 29th of November 1774.
2. In terms of the composition of the project team, the following consultancies have been working with the Applicant in formulating proposals for Whitney-on-Wye Toll Bridge as follows:
 - RRA Architects – Hereford office: architects responsible for providing architectural services, lead consultant, building heritage strategy advice and designing the application proposals, in close consultation with the Applicant and project team.
 - James Johnston – Responsible for Ecology survey and bat mitigation advice.
 - Jerry Ross – Arboricultural survey and advice.
3. This Statement of Need (SN), is written by the Applicant in their own words and sets out a comprehensive assessment of the development proposals and makes a clear and unequivocal case for why planning permission should be granted in accordance with the provisions of current planning policy and Act of Parliament dated 1774. In so doing, the statement conforms to the preserved UDP policies as well as the requirements of The NPPF and the material considerations of emerging policy within the LDF. With this in mind, this SN accompanies the Listed Building Consent Application and Outline Planning Application and is structured as follows:
 - Section 2: THE SITE AND HISTORY
 - Section 3: CURRENT SITUATION AND NEED
 - Section 4: THE FUTURE - SECURED

Whitney-on-Wye Bridge – A Proposal

Justification Statement of Need

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- Section 5: APPENDIX

4. In addition to this SN, there are a number of supplementary reports prepared by the Applicant. Where relevant, these are referred to in the SN.

Executive Summary

It is considered that the development proposals contained within this Listed Building Consent Application and Outline Planning Application accords with – the authority enacted by Parliament in 1774, the preserved policies of the Unitary Development Plan, the National Planning Policy Framework and the emerging policies outlined within the LDF.

In so doing the proposed development and alterations to a Listed Building are required in order that the site continues to maintain the Toll Bridge at Whitney-on-Wye in perpetuity.

In addition the proposed development:

- sustains and enhances the SSSI;
- provides employment;
- sustains and enhances the Bridge and Toll House well into the future;
- does not affect agricultural land;
- has the support of Natural England;
- has the support of the Parish Council;
- has the support from the local community;
- has the support from the Local Member for Herefordshire Council;
- is not financially supported by Hereford Council;
- is a vital local infrastructure need;
- sustains and enhances tourism within the local area;
- is privately funded and not a burden to the tax payer.

Whitney-on-Wye Bridge – A Proposal

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SECTION 2: THE SITE AND HISTORY

5. The area associated with the LBC application and the proposed outline planning application is Whitney-on-Wye Toll Bridge, Toll Bridge Cottage and associated land and river. (Total Area 0.4753 hectares).

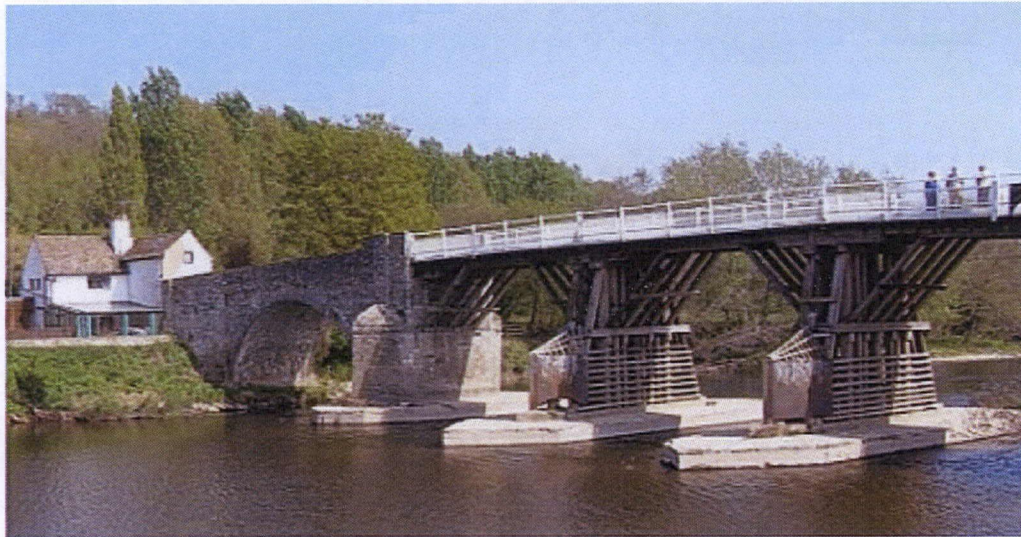


Fig 2 – The Whitney-on-Wye Toll Bridge looking East

6. The Bridge is Grade 2 listed and governed by the Act of Parliament dated 29th November 1774. At no time has the Act been superseded or removed.
7. The land and Bridge does not fall within a Conservation Area and the SSSI does not extend over the Bridge, or the majority of land – the SSSI extends to 10m back from the edge of the riverbank. The Arboriculture Survey, states... *'The belt of trees along the riverbank are in fair condition but are unexceptional specimens. They consist of self-sown Alder, Sycamore, and Birch and are perfectly in keeping with the riverine environment. Given their inclusion within the SSSI it would be preferable if these were not affected by development proposals'*... the proposed development does not affect the SSSI.
8. The land and Bridge sits within the River Wye SAC. The Ecology Survey, states... *'No evidence was found of any protected fauna inhabiting the application site, and similarly the potential for notable fauna is negligible, based on the habitat types present and lack of suitable opportunity. The areas proposed for redevelopment are considered of low ecological value'*... The proposed development seeks to improve the ecological value by

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removing the tarmac road surface from within the river side areas and introducing green roofing on all proposed low level buildings – to promote bio-diversity.

9. The Toll Bridge Cottage and Toll House are Grade 2 listed and were built under the authority of the Act of Parliament dated 29th November 1774, in order to build and support the upkeep of the Bridge... *and to keep the same in good repair forever.*
10. The land was allocated under the authority of the Act in order to support the function and operation of the Toll Bridge and to ensure its sustainability into the future and with a special proviso that the Toll Master may... *inclose a piece of ground... to build a toll house... and to make use of the remainder... for such other purpose as they shall think proper...for ever.*
11. There is also another clause within the Act, which further enacted... *that it shall... be lawful for the said Undertakers, their heirs and assigns... to take and carry away, such quantity of stone as they shall think proper... within the said manors, for the purpose only of repairing the said bridge and Tollhouse.*

Undertakers
may inclose
a Piece of
Common for
Building a
Toll House
etc.

And be it further enacted by the Authority aforesaid,
That it shall and may be lawful for the said Undertakers,
their heirs and assigns, to inclose a piece of Ground,
Part of the Commons and Waste lands within the said
Manors, contiguous to the said Bridge, not exceeding
One Acre, and to build a Toll House upon part thereof,
and to make use of the remainder for a yard and garden
to such Toll house, or for such other purposes as they
shall think proper; and when the said piece of ground
shall be so inclosed, the same shall be, and is hereby
vested in the said Undertakers.

Fig 3 – Extract from the 1774 Act of Parliament

12. The Toll Bridge was initially owned by the same family, for over 200 years until 1981, when it was bought for the first time by someone outside the original family. Since 1981 the Bridge was sold in 1990, 1993, 2002 and then finally to us – Maggie Taylor & Grahame Penny, in 2012. We understand that all of the transfer of ownerships since 1990 has been primarily due to the uneconomic nature of the Bridge maintenance and upkeep costs versus Toll income.
13. Originally there were two structures located at the site; a Toll Bridge Cottage, and the Toll House, (see picture dated 1940).

Whitney-on-Wye Bridge – A Proposal

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Fig 4 – The Whitney-on-Wye Toll Bridge dated 1940

14. The Toll Bridge Cottage on the left exists today and forms part of the LBC application for internal refurbishment and minor external alterations, returning the original Toll Bridge Cottage to a more sympathetic state of appearance. The Toll House, pictured on the right, burnt down in the 1940s and its replacement now forms part of the outline planning application for rebuilding, in order to support income generating potential at the site – in accordance with the 1774 Act.
15. The main road was redirected in 1970s and was positioned as can be seen below in photograph taken in 1969.

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Fig 5 – The Whitney-on-Wye Toll Bridge dated 1969

16. The outline planning application contains Eco-lodges, to be used as fishing lodges, to be positioned where the old road was located. This has been endorsed by Katey Stephen of Natural England, who confirms in her email dated 31st July 2012 that... *'If the land was to go down to grassland and the buildings were stilted this may be regarded as an enhancement to the land, from its current state'...* As this piece of land was always designated as Bridge land, and subsequently the Old Road and the New Bridge abutment, it has never been designated as agricultural land. As you will see from the Ecology survey described above, the recommendation to enhance ecology at the site is also for... *'Removal of the tarmac Old Road'*.

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Fig 6 / 7 – The Whitney-on-Wye Toll Bridge as it appears today

17. Whitney Bridge is designated by The Department of Transport as... *a not for profit private entity*... providing a public function, which would ordinarily be performed by Local or Central Government. All Toll increases need to be approved by Department of

Whitney-on-Wye Bridge – A Proposal

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Transport and ratified by Parliament, and all provisions under the 1774 Act of Parliament, by which the Bridge was conceived, have not been repealed. Ref: <http://www.dft.gov.uk/publications/statutory-tolled-undertakings-in-the-uk/>

18. As a public entity we have the responsibility under the 1774 Act to remain open for public crossings – 24 hours a day – 365 days per year, plus we provide an essential passage for all Emergency services to attend call-outs. There have been approx. ten emergency crossings in the past year, between 2am and 6am, for which we have to awake and raise the barrier – this, of course, is a life and death situation.
19. The purpose of the 1774 Act is to protect the Bridge in perpetuity for the community past, present and future. Our understanding is that Herefordshire Council were offered an ability to purchase the Whitney-on-Bridge in the 1960s, and again more recently, therefore bringing the Bridge into public ownership, however Herefordshire Council declined – we understand due to economic profitability reasons – balancing Toll income with staff management and future maintenance needs.
20. Toll fees were increased in 2009 from 50p – 80p, which caused a major problem for many local businesses at this time – so as the current owners we have introduced a series of reduced season tickets for local regular users. Our business plan for future sustainability of Whitney-on-Wye Bridge cannot be based on a theory of steadily increasing Toll Charges as this is proven to be untenable – as the maintenance costs of an ancient Bridge is already out stripping inflation. In addition we do not wish to burden local / regular users with increased charges, instead we intend to focus our plans on drawing more visitors/tourists to the area, to help sustain the community both for income and also for employment opportunities and to help sustain the Bridge in accordance with the 1774 Act.

SECTION 3: CURRENT SITUATION AND NEED

21. The balance of income versus maintenance and management of Whitney Bridge has become increasingly difficult over the last few years with the changes in VAT rate (15% - 20%), cost of Oak increasing, cost of fuel raising maintenance bills, increased electricity costs, increase in minimum wage and associated legislation, increased insurance costs due to IPTax, plus the change in the 2012 Budget which removed the exemption for Grade 2 listed structures from VAT liability. We are also very aware that

Whitney-on-Wye Bridge – A Proposal

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Whitney-on-Wye Bridge has a future lifespan of 20 – 25 years in its current condition, which would necessitate a rebuild in / around 2035. At present there has not been any provision made for a rebuild (set at £1.5m for insurance purposes), which means we need to execute and have in place a robust business plan, which cater for this eventuality and includes on-going general annual maintenance, which will prolong the lifespan of the current structure and introduce savings.

22. Toll income in isolation will NOT provide for maintenance and a re-build fund.

Toll income figures

Up until 2012 we only have access to turnover figures as there was not a requirement to provide audited annual accounts – we have however produced annual audited accounts and will do so going forward: –

Year end	Total income from Toll charges
2007	£45899.27
2008	£48585.34
2009	£48827.19
2010 (includes 2009 toll increase)	£66039.84
2011	£63386.11
2012	£75737

So far in 2013 we are this position

2013	£61781.80 more or less identical to 2012
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23. Reasons for increased income 2011 / 2012 – increased PR / Media attention for visitors' attraction plus ice cream / coffee sales to visitors. Reason for decreased income for 2013 so far comparing year on year – weather! The Bridge has been closed due to impassable local roads with snow and flooding for approx. 9 days in total during 2013. Plus with the bitterly cold weather (prior to the hot spell), tourists are not going to Hay-on-Wye for their day out to the same extent as they did prior to. The local Golf Club has been closed and one of their tenants, Highground Garden Maintenance, was evicted from the site – these two events have cost us £400 per month in income.
24. We have attached a copy of 2012 accounts (See Appendix A), which shows the breakdown of insurance, maintenance etc. plus a very minimal salary for the Toll Masters – these show a £3463 loss for 2012, without maintenance, or rebuild provision being made. In addition to this we have only been able to support ourselves through our

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- personal savings, rather than a salary, and the development proposed is being funded, again, by us completely personally from savings. We feel without this personal investment such as ours and support from Herefordshire Council etc Whitney-on-Wye Bridge will be untenable within 3 – 5 years, and it will be forced to shut.
25. We have total support from the Parish Council and from within the local community (See Appendix B).
26. We have already made huge inroads to minimise losses for 2013 – we have renegotiated the insurance premium to £3,900 instead of £14,006 – by reducing flood risk on The Toll Bridge Cottage by looking at changing the layout, which is part of the LBC application attached. There will hopefully, be reduced legal costs, as these have been incurred during the year 2012 – with Herefordshire Council's challenge to the legality of the 1774 Act of Parliament's exemption to Council and Business Tax liabilities at Whitney-on-Wye Bridge and Toll House. However we also know that general maintenance will increase this year as a direct impact from continued frost/cold damage to the Oak curbs on the surface of the bridge – the quote for this work is £9750 + vat.
27. For the avoidance of doubt without investment, such as ours, and the support from Herefordshire Council Planning etc. Whitney-on-Wye Bridge will be untenable within 3 – 5 years and will be forced to close, which will have a direct major impact on local businesses and schools. On a daily basis we have approx. 350 car passages (approx. 120 average payers, as the cost of 80p covers any number of crossings for one person per day). We have 150 annual season tickets holders in operation at a cost of £120 per family of two cars. On average some season ticket holders will cross the bridge up to 8 times a day, going to and from, work, home, school, lunch break, supermarket, petrol etc – should the bridge close the total cost of this for them to drive around a different route would be £28 per day with the current cost of petrol/diesel – the demise of Whitney-on-Wye Bridge would constitute serious threats to the sustainability of the local community, both economically and time constraints.
28. Given the current financial position the Bridge needs to find an alternative revenue stream in order for Whitney-on-Wye Bridge to be sustained into the future for the community in perpetuity – this is dependent on improved tourism facilities, increased visitor numbers and increased revenue opportunity being generated from within the site as the 1774 Act allows.

Whitney-on-Wye Bridge – A Proposal

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29. We have already applied for, and been awarded, a liquor license and all the Health and Hygiene compliancy to add value to camping events by offering BBQ and breakfast options. We have also printed guidebooks and postcards to give to tourists to encourage advertising and marketing. We have employed a PR media consultant to gain better access to free newspaper media in order to create more events here. We have an eBay site to retail specialist coins we are given both in modern currency and with foreign currency. We have worked tirelessly with Trip Advisor and Facebook to create a community to increase customer traffic and awareness for camping, fishing and general tourism. We have also spent many hours working with SatNav systems to ensure traffic is informed as to the correct most effective route to Hay on Wye.
30. We have produced a new website with a new designed brand / logo www.WhitneyBridge.co.uk and worked hard with Google to ensure our camping and fishing is represented effectively on their listings. We have secured a lease for fishing for the 2013 Coarse Fishing Season with Bute Fishing Club for their Annual Competition events.
31. All of which undoubtedly produced the £10K increase in revenue from 2011 to 2012.
32. Charity Days – Since January 2012 as new owners of Whitney-on-Wye Toll Bridge and it's lands we have offered 7 local charities the opportunity to run the Toll Booth for the day, and for them to keep the money for their own funds. We also offered the use of the land for other activities for fund raising on those days too. The days have been very successful for all the charities concerned, and to date approx. £2500 has been raised between them. For the local charities not only does it offer them an opportunity to collect much needed funds but it also allows them to understand the principles and workings behind the Toll Bridge and its presence and importance within the community. For Whitney-on-Wye Bridge the Charity Days provide much needed local social interaction, which is hard to put a price on, Of course the Charity Days affect the income levels for the Bridge, but the local social interaction and educational element of these is deemed invaluable to sustain the Bridge within the community into the future.

SECTION 4: THE FUTURE – SECURED

33. The proposed business plan includes the following facilities as defined in the outline planning application, which consists of:

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1. Four Eco-lodges used as fishing lodges (sleep 2, single storey, minimal low impact foundation design);
 2. LBC refurbishment and alteration of the existing Old Toll Bridge Cottage, which will be a home for us whilst works on the New Toll House are completed – then the old Toll Bridge Cottage will become a self-contained letting unit;
 3. New Toll House for the Toll Master's House (2 bedrooms, 2 bathrooms, 2 storey to replace second house burnt down in the 1940's but set back on land to ensure it is outside the flood zone and the SSSI;
 4. A Garage, Store, Ablutions and Visitors Kiosk to provide improved facilities.
34. The budget for the development work as above is as follows (funded privately).

Item	Budget
Lodges x 4	£160,000
Refurbishment Cottage	£30,000
New Toll House	£120,000*
Sundries	£5,000
Fitout	£15,000
Total	£330,000
Notes	*Huff House @ £1500 per square metre

35. Total budgeted income generated from lodges @ 40 weeks per year at average of £300 per week (over all season) £48K per year. Toll Bridge Cottage income would be in addition to this @40 weeks per year at average £600 per week (over all season) £24K per year.
36. Income from visitors for 40 weeks per year @20 people per day for 6 days per week spending £5 per head: £24K per year.
37. Combined revenue from tourism circa £96k per annum. (Appendix C)

Whitney-on-Wye Bridge – A Proposal
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SECTION 5:

APPENDIX A

TAYPEN 02 LTD

Profit and loss account for the year ended 31 December 2012

	<u>2012</u> £	%	<u>2011</u> £	%
Sales				
Sales - United Kingdom	-	-	115,897	100.0
Fines	3,300	4.4	-	-
Season ticket income	6,329	8.4	-	-
Toll charges income	61,992	81.9	-	-
Sundry sales	4,116	5.4	-	-
	<u>75,737</u>	<u>100.0</u>	<u>115,897</u>	<u>100.0</u>
Cost of sales				
Opening stock and work in progress	-	-	35,000	30.2
Purchases	-	-	18,727	16.2
Sundry purchases	855	1.1	-	-
Food purchases	1,643	2.2	-	-
Subcontractors	1,285	1.7	-	-
Carriage and packing	-	-	13,096	11.3
	<u>3,783</u>	<u>5.0</u>	<u>66,823</u>	<u>57.7</u>
Gross profit	71,954	95.0	49,074	42.3
Deduct: Expenses				
Rent & service charges	110	0.1	9,174	7.9
Premises licence	100	0.1	-	-
Rates	215	0.3	-	-
Light and heat	2,466	3.3	-	-
Insurance	14,006	18.5	519	0.4
Cleaning	329	0.4	-	-
Repairs and maintenance	4,389	5.8	451	0.4
Emoluments for services as directors	5,400	7.1	-	-
Salaries	5,520	7.3	95	0.1
Telephone	688	0.9	787	0.7
Design & research costs	-	-	384	0.3
Stationery & postage	583	0.8	1,211	1.0
Sundries	298	0.4	286	0.2
Computer costs	1,026	1.4	1,394	1.2
Advertising & promotion	100	0.1	-	-
Motor expenses	1,569	2.1	500	0.4
Travelling	-	-	2,326	2.0
Entertaining	-	-	167	0.1
Advertising and promotion	5,850	7.7	668	0.6
Loan interest - within five years	1,218	1.6	2,124	1.8
Credit card charges	149	0.2	7,304	6.3
Bank charges and interest	3,553	4.7	729	0.6
Mortgage interest	10,219	13.5	-	-
Bad debts	1,036	1.4	90	0.1
Accountancy charges	2,930	3.9	7,775	6.7
Legal & professional charges	12,644	16.7	7,169	6.2
Depreciation of plant and machinery	785	1.0	480	0.4
Depreciation of fixtures and fittings	234	0.3	532	0.5
Fixed asset investment written off	-	-	100	0.1
	<u>75,417</u>	<u>99.6</u>	<u>44,265</u>	<u>38.2</u>

TAYPEN 02 LTD

Profit and loss account for the year ended 31 December 2012 (continued)

	<u>2012</u> £	%	<u>2011</u> £	%
	<u>(3,463)</u>	<u>(4.6)</u>	<u>4,809</u>	<u>4.1</u>
Exceptional items				
Loss on sale of assest in discontinued operations	<u>-</u>	<u>-</u>	<u>(240,889)</u>	<u>(207.8)</u>
Net loss/(2011 profit) before taxation	<u><u>(3,463)</u></u>	<u><u>(4.6)</u></u>	<u><u>245,698</u></u>	<u><u>212.0</u></u>

Whitney-on-Wye Bridge – A Proposal
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SECTION 5:

APPENDIX B

Whitney Toll Bridge, Whitney-on-Wye, Herefordshire

The closure of Whitney Toll Bridge would mean inconvenience to my journey to and from my place of work near Clifford. I would have to travel an extra 6 miles each day plus incur the added costs of fuel. I work 5 days a week so this would mean at least 30 extra miles traveling every week.

*The present custodians of the bridge should be allowed to develop this unique and historic point of interest in a sympathetic way. The bridge is a tourist attraction in itself and sensitive development would help safeguard the bridge's future by allowing extra income and investment in maintaining this exceptional and important historic structure. **Mark Rogers, 4 Hill Corner Cottage Ledgemoor Herefordshire HR4 8QG***

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*Whitney On Wye plays a very important part In Our day to day working life, without this bridge we would have to travel around 14 miles extra and apart from the added time the fuel costs would be another burden!!!! We use the bridge anything from 4 to 10 times a day because we travel between the two family farms and it is essential that it remains open for years to come. **Michael Pugh, Sheepcote, Farm, Clifford, Herefordshire HR3 5HU***

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I am fully supportive of the application from the owners of the bridge. Should the bridge fail to operate then it would cause the locals and visitors to this area a great deal of hardship. It is a very well used route that would change people's outlook on going out should it not be available. Older people especially would find it financially hard and maybe unmanageable to drive the extra miles to reach destinations.

This road is our main access to amenities in the area such as shops, doctors, schools, nurseries, pharmacies, vets etc. The bridge brings in much needed visitors to the area and thus local retailers benefit also.

*Emergency services use this bridge on a regular basis and would find it adds a great deal of time to their stretched network should it not be operational. I would greatly miss the opportunities that it brings me if it were not there. **Glen Toms***

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Re: Whitney Toll Bridge

*We as a family wish to emphasise the value of the above crossing over the river Wye. Most of our journeys take us over the bridge in order to access the A438. If this was not available, it would mean travelling an extra 8 miles to access the nearest bridge (a total of 16 miles on each return journey) Therefore Whitney bridge crossing saves us in both time and money (a considerable amount over the period of a year) This factor obviously applies to all local residents in particular the agricultural community of which we are part, affording farmers access to land on either side of the river, thereby avoiding further congestion on the A438 trunk road. Families with children also need speedy access to the A438 in order to get children to secondary education. **Signed: Clive Stokes, Sue Stokes, Adrian Stokes***

Continued...

We are a local family from the town of Hay-on-wye. we moved to the village of Clifford over 30 years ago, we continue to live in numerous houses in the area ever since. For us Whitney Bridge is a LIFE LINE! We probably cross the bridge nearly every day, sometimes several times a day, for various reasons, both for Work and pleasure.

If the bridge were to close, it would create quite a hardship for our family. We would then have to travel back to Hay, then through Clyro to reach Whitney. Not only would the closure make life very difficult, it would be a great loss of a beautiful old bridge.

The bridge is a big part of the community. The Whitney Toll Bridge now has new owners. I feel they have made the bridge even more a vital part of the community. I feel they have created a very special little place, both for locals and visitors alike. We can now all stop for a welcome little break. We can enjoy a coffee and watch the world go by. The new owners have also helped quite a few local charities, giving up their daily takings.

One other point I must make, they have also given locals reduced rates to cross the bridge, this is very much appreciated by all. For our family, like so many others in the area the closure of Whitney Toll Bridge would be an absolute DISASTER. **John & June Jones Castle View Clifford Hereford HR3 5EX**

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I understand that the current owners of the Toll Bridge are looking for planning permission to enable them to develop their site. Should the bridge fail to operate then it would cause the locals considerable difficulties and a very circuitous route should they wish to get to Clifford, Hay, Hardwicke and other local areas.

For locals to access the nursery schools, play schools, doctor's surgeries, shops, post office, pharmacy and friends this could involve another extra 8 miles (at least), to their journey. This is at a time of increasing oil/insurance/service charges for people who use their cars. Emergency vehicles, for whom time is crucial, frequently use the bridge to get to a main road (Hereford to Brecon road), that accesses many villages and towns.

We have no local transport that we can use so private cars or Dial A Ride is a necessity and they all use the bridge. Local farmers also have to use this route to enable them to get to animals/arable fields / vegetable fields - sometimes in very short spaces of time when the river is liable to flood, which it is doing more and more.

Apart from the necessities - do we really want to lose such a historical and quirky asset to our locale? It brings in huge amounts of interest and financial gain for the local retailers from visitors to the area. We not only have national but international interest and all of these people spend a great deal of money locally.

I would ask that you take these points into account when considering your decisions.

Alison Toms [<mailto:alison.glen@hotmail.co.uk>]

Whitney-on-Wye Bridge – A Proposal
Justification Statement of Need
September 2013

SECTION 5:

APPENDIX C

Income Expenditure Analysis for Lodges and Toll Cottage

Build costs

Refurb Cottage	£	30,000.00
Build Tollhouse	£	120,000.00
Build 4 x Lodges @40k	£	160,000.00
Total	£	310,000.00

Income

Based on 40 weeks per year

4 Lodges at £300 per week	£	48,000.00
Toll Cottage @ £600 per week	£	24,000.00
visitor spend 20 people £5/week	£	24,000.00
Total	£	96,000.00

Running costs

Electric @£400/month	£	4,800.00
Water @£150/month	£	1,800.00
Cleaning @£100/week	£	5,200.00
Consumables @£10/week	£	400.00
Waste Removal @£200/year	£	200.00
Maintenance @ £500/unit/year	£	2,500.00
Total	£	14,900.00

Net Annual income	£	81,100.00
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Condensed P&L Projection

All figures at 2013 values - no inflation

	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035	2036	2037	2038
Sales																												
Tollbridge	75737	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000	80000
Lodge Letting	0	0	0	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000	48000
Toll House Letting	0	0	0	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000
Visitor spend	0	0	0	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000	24000
Total Income	75737	80000	80000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000	176000
Cost of Sales	3783	4000	4000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000	9000
Gross Profit	71954	76000	76000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000	167000
Expenses	75417	76000	76000	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800	106800
Loan Repayment				20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000	20000
Maintenance				10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000	10000
Major Maintenance Fund				30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000
Total Expenses	75417	76000	76000	166800	166800	166800	166800	166800	166800	166800	166800	166800	155800	146800	146800	146800	146800	146800	146800	146800	146800	146800	146800	146800	146800	146800	146800	146800
Net Profit (Loss)	-3463	0	0	200	200	200	200	200	200	200	200	200	11200	20200	20200	20200	20200	20200	20200	20200	20200	20200	20200	20200	20200	20200	20200	20200
Loan			190000	170000	150000	130000	110000	90000	70000	50000	30000	10000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000	1000

Notes

2012/2013 Salary and Directors Emoluments total £10,920 increased to £40k for 2013 (£20k each for M Taylor and G penny)

2012/2013 Insurance cost £14006 reduced to £3600 ongoing

2012/2013 Legal costs reduced from £12644 to zero

2014 Investment of £190k for cottage/Lodges repaid by 2024 (10 years, 0% interest)

Maintenance Fund	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	2025	2026	2027	2028	2029	2030	2031	2032	2033	2034	2035	2036	2037	2038
Contribution	0	0	0	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000	30000
Total				30000	60000	90000	120000	150000	180000	210000	240000	270000	300000	330000	360000	390000	420000	450000	480000	510000	540000	570000	600000	630000	660000	690000	720000	750000